

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

**OFFICE OF DESIGN POLICY & SUPPORT
INTERDEPARTMENTAL CORRESPONDENCE**

FILE P.I. # 0013549 **OFFICE** Design Policy & Support
Chatham County
GDOT District 5 - Jesup **DATE** 07/14/2016
Pedestrian Bridge over SR21 in Port Wentworth

FROM  Brent Story, State Design Policy Engineer

TO SEE DISTRIBUTION

SUBJECT APPROVED CONCEPT REPORT

Attached is the approved Concept Report for the above subject project.

Attachment

DISTRIBUTION:

Hiral Patel, Director of Engineering
Joe Carpenter, Director of P3/Program Delivery
Genetha Rice-Singleton, Assistant Director of P3/Program Delivery
Albert Shelby, State Program Delivery Engineer
Darryl VanMeter, State Innovative Delivery Engineer
Bobby Hilliard, Program Control Administrator
Cindy VanDyke, State Transportation Planning Administrator
Eric Duff, State Environmental Administrator
Bill DuVall, State Bridge Engineer
Andrew Heath, State Traffic Engineer
Angela Robinson, Financial Management Administrator
Lisa Myers, State Project Review Engineer
Monica Flournoy, State Materials and Testing Administrator
Lee Upkins, State Utilities Engineer
Paul Tanner, State Transportation Data Administrator
Attn: Systems & Classification Branch
Richard Cobb, Statewide Location Bureau Chief
Ed David Adams, State Safety Program Manager
Brad Saxon, District Engineer
Troy Pittman, District Preconstruction Engineer
Dallory Rozier, District Utilities Engineer
Tim Matthews, Project Manager
BOARD MEMBER - 1st Congressional District

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

LIMITED SCOPE PROJECT CONCEPT REPORT

Project Type: Ped Bridge & Sidewalk P.I. Number: 0013549
GDOT District: 5 County: Chatham
Federal Route Number: N/A State Route Number: SR 21
Project Number: _____

Construct a pedestrian bridge crossing over State Route 21 in Port Wentworth (Chatham County), Georgia which will serve the population of a growing residential neighborhood and a new elementary school. No impacts will be made to the State Route 21 roadway as part of this project.

Submitted for approval:

[Signature], ARCADIS 3/29/2016
Consultant Designer & Firm or GDOT Concept/Design Phase Office Head & Office Date

(N/A)
Local Government Sponsor Date

[Signature]
State Innovative Delivery Administrator 3-30-2016
Date

[Signature]
GDOT Project Manager 03/29/2016
Date

Recommendation for approval:

ERIC DUFF*/EKP 6/10/2016
State Environmental Administrator Date

State Traffic Engineer Date

BILL DUVALL*/EKP 4/6/2016
State Bridge Engineer Date

- MPO Area: This project is consistent with the MPO adopted Regional Transportation Plan (RTP)/Long Range Transportation Plan (LRTP).
- Rural Area: This project is consistent with the goals outlined in the Statewide Transportation Plan (SWTP) and/or is included in the State Transportation Improvement Program (STIP).

CINDY VANDUYKE*/EKP 4/6/2016
State Transportation Planning Administrator Date

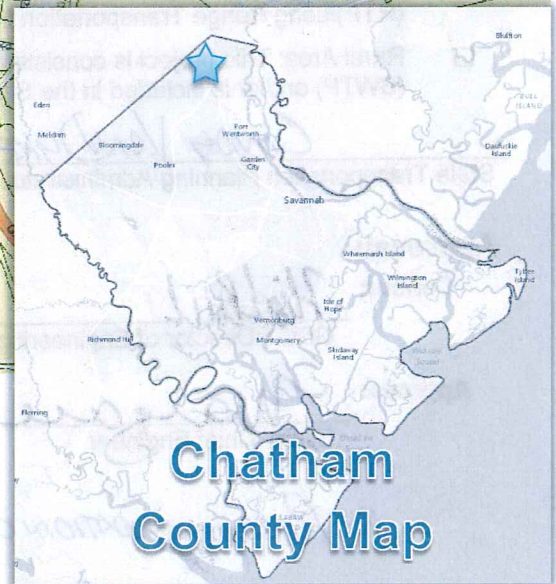
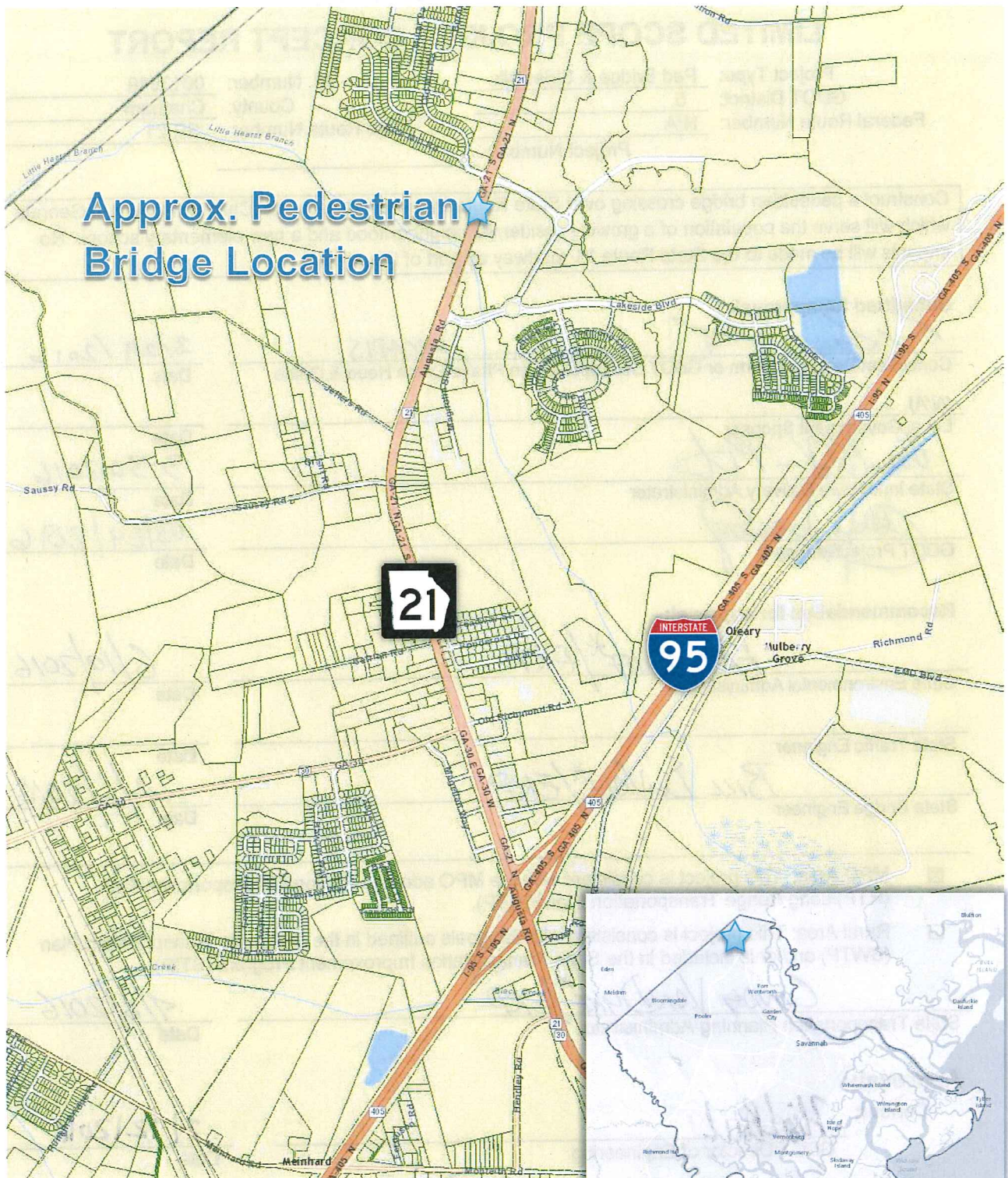
Approval:

Concur: [Signature] 7/12/2016
GDOT Director of Engineering Date

Approve: Margaret B. Pickle 7.12.16
GDOT Chief Engineer Date

*- RECOMMENDATION ON FILE

PROJECT LOCATION



County: Chatham

PLANNING & BACKGROUND DATA

Project Justification Statement: The need for this project is to provide ADA compliant pedestrian access to/from the Rice Creek School to/from the developing residential communities within the vicinity of the Rice Mill Road signalized intersection with SR 21. Residents will be able to use the crossing to more safely cross SR 21 to take their children to school instead of driving, and access goods and services provided by the Rice Creek Subdivision.

Existing conditions: State Route 21 is a four-lane facility with a depressed grassed median and left turn lanes at the intersection.

Other projects in the area:

- 0012722 – SR 21 @ I-95 Diverging Diamond
- 0011743 – SR 21 Ffrom I-516 to Effingham County Line – Corridor Study
- M003685 – Chatham SR 21 Intersection Improvements

Description of the proposed project:

The Project will install a pedestrian bridge over SR 21 near the intersection of Rice Mill Road / Market Boulevard at SR 21. The Pedestrian Bridge across SR 21 will have a minimum clear width of ten feet (10'). The pedestrian bridge will span all existing lanes of SR 21. Access ramps meeting ADA standards will be installed at both termini of the pedestrian bridge. Sidewalks will be constructed along SR 21 in front of the Access Ramps and will extend north to the Rice Creek neighborhood. All sidewalks will be designed to accommodate both pedestrian and bicycle usage. No modifications to the existing SR 21 will be required.

The Project length is approximately 0.12 miles along SR 21. The project site is located approximately two (2) miles north of the interchange of I-95 and SR 21. Existing SR 21 right-of-way (ROW) is approximately two-hundred (200) feet wide. Additional ROW and easements will be procured by GDOT for the construction of the access ramps.

MPO: Chatham Urban Transportation Study (CUTS)

TIP #: (N/A)

TIA Regional Commission: Central Savannah River RC

Congressional District(s): 1

Federal Oversight: Exempt State Funded Other

Projected Traffic: 24 HR T: 8.22%
 Current Year (2014): 31,400 AADT
 Traffic Projections Performed by: GDOT Permanent Station 051-0109

Functional Classification (Mainline): Urban Principal Arterial

Complete Streets - Bicycle, Pedestrian, and/or Transit Standards Warrants:

Warrants met: None Bicycle Pedestrian Transit

Pavement Evaluation and Recommendations

Preliminary Pavement Evaluation Summary Report Required? No Yes
 Preliminary Pavement Type Selection Report Required? No Yes
 Feasible Pavement Alternatives: HMA PCC HMA & PCC

DESIGN AND STRUCTURAL

Description of Proposed Project:

Major Structures: A ten foot (10') wide Pedestrian bridge will span all existing lanes and the clear zone for SR 21, just south of the intersection with Rice Mill Road and SR 21.

There are no existing structures present.

Mainline Design Features: Beginning at the western side of the radius return of the southwest quadrant of the intersection, a ten foot (10') sidewalk, with a grassed buffer, will extend south along SR 21 to the southernmost extent of the Pedestrian Bridge construction. ADA compliant Pedestrian Bridge Access Ramps, ten feet (10') in width, will be installed on either side of the Pedestrian Bridge. Along the eastern side of SR 21, the ten foot (10') sidewalk will extend the length of the access ramp construction and a ten foot (10') wide sidewalk will be installed to within five feet (5') of the Rice Creek School Right of Way. Rice Creek School will install a direction connection from the school to the proposed pedestrian facilities.

The existing lanes of SR 21 will not be modified or impacted by the project. If a median pier is necessary, crash protection will be provided within the median. All Access Ramp and Pedestrian Bridge components will be constructed outside the SR 21 Clear Zone.

| Feature | Existing | Standard* | Proposed |
|---|-------------------------|-------------------------|--|
| Typical Section | | | |
| - Number of Lanes | 4 | 4 | No Modifications |
| - Lane Width(s) | 12-ft | 12-ft | No Modifications |
| - Median Width & Type | 40-ft Depressed Grassed | 40-ft Depressed Grassed | No Modifications |
| - Outside Shoulder or Border Area Width | 5-ft paved, 10-ft total | | 30-in C&G, variable Grassed Buffer, 10-ft sidewalk |
| - Outside Shoulder Slope | Varies | 4:1 | 2% to match sidewalk slope |
| - Inside Shoulder Width | 3-ft paved | | No Modifications |
| - Sidewalks | N/A | 5-ft | 10-ft |
| Posted Speed | 55 mph | | 55 mph |
| Design Speed | 55 mph | | 55 mph |

*According to current GDOT design policy if applicable

Major Interchanges/Intersections:

Lighting required: No Yes

Transportation Management Plan [TMP] Required: No Yes
 If Yes: Project classified as: Non-Significant Significant
 TMP Components Anticipated: TTC TO PI

Will Context Sensitive Solutions procedures be utilized? No Yes

Design Exceptions to FHWA/AASHTO controlling criteria anticipated: N/A

Design Variances to GDOT Standard Criteria anticipated: N/A

UTILITY AND PROPERTY

Temporary State Route Needed: No Yes Undetermined

Railroad Involvement: N/A

Utility Involvements:

The following utility companies are located within the project limits and are being engaged for coordination/relocations:

- City of Port Wentworth
- AT&T
- Georgia Power Distribution
- Georgia Power Transmission
- Comcast CATV
- Hargray Communications
- Atlanta Gas Light

SUE Required: No Yes

SUE Approved: March 2, 2016

Public Interest Determination Policy and Procedure recommended? No Yes

Right-of-Way: Existing width: Approx. 185 ft. Proposed width: Varies, 275 ft. max

Required Right-of-Way anticipated: No Yes Undetermined

Easements anticipated: None Temporary Permanent Utility Other

Anticipated total number of impacted parcels: 2
 Displacements anticipated: Businesses: 1
 Residences: 0
 Other: 1
 Total Displacements: 0

ENVIRONMENTAL AND PERMITS

Anticipated Environmental Document:

GEPA: **NEPA:** CE PCE NONE ^{EKP}

MS4 Compliance – Is the project located in an MS4 area? No Yes

Environmental Permits, Variances, Commitments, and Coordination anticipated:

Air Quality:

Is the project located in a PM 2.5 Non-attainment area? No Yes

Is the project located in an Ozone Non-attainment area? No Yes

Carbon Monoxide hotspot analysis: Required Not Required TBD

Air Quality is expected to clear the Air Analysis with a write off letter.

USACE Permit: The Design-Build team will be responsible for all applicable environmental permits and stream buffer variance applications, as needed.

NEPA/GEPA Comments & Information: (Describe anticipated effects to ecology, history, archeology, air quality, noise effects, public involvement, etc. & the potential effect on the environmental document)

GEPA: ~~GEPA Type B~~ TOTAL PROJECT COST IS LESS THAN \$100 MILLION, SO A GEPA DOCUMENT IS NOT REQUIRED. (EP)

Ecology: ERSR-AOE, nationwide permit anticipated

History: There are no historic properties within the construction limits of the project.

Archeology: There are no archaeological properties within the construction limits of the project

Noise Effects: NA, per GEPA policy

Major Stakeholders: City of Port Wentworth, Savannah-Chatham County Public School System

COORDINATION, ACTIVITIES, RESPONSIBILITIES, AND COSTS

Project Meetings:

GEC Coordination / Concept Development Meetings:

- January 29, 2016
- February 19, 2016
- February 26, 2016
- March 19, 2016

Utility Owners Outreach: February 23, 2016, Savannah Georgia, Meeting Minutes Attached

SR 21 Pedestrian Bridge Industry Forum: March 1, 2016

RFP Review Meeting: March 31, 2016

| Project Activity | Party Responsible for Performing Task(s) |
|---|--|
| Concept Development | GDOT |
| Design | Design-Build Team |
| Right-of-Way Acquisition | GDOT |
| Utility Coordination (Preconstruction) | GDOT |
| Utility Relocation (Construction) | Design-Build Team |
| Letting to Contract | GDOT |
| Construction Supervision | GDOT |
| Providing Material Pits | Design-Build Team |
| Providing Detours | Design-Build Team |
| Environmental Studies, Documents, & Permits | GDOT |
| Environmental Mitigation | GDOT |
| Construction Inspection & Materials Testing | GDOT |

Other coordination to date:

Project Cost Estimate and Funding Responsibilities:

| | Breakdown of PE | CST* | Total Cost |
|------------------|--------------------------------------|---|----------------------------------|
| Funded By | GDOT <i>KP</i> | GDOT <i>KP</i> | \$4,218,812 <i>KP</i> |
| \$ Amount | \$115,000 <i>190,701</i> | \$3,986,476 <i>4,028,111</i> | \$4,101,476 |
| Date of Estimate | 02/17/2016 <i>4/13/16</i> | 05/05/2016 <i>4/13/16</i> | |

*CST Cost includes: Construction, Engineering and Inspection, Right of Way, Reimbursable Utility, Environmental Mitigation, and Contingencies.

ALTERNATIVES DISCUSSION

Preferred Alternative: Construct a Pedestrian Bridge with Access Ramps that spans all lanes of SR 21 as well as the Clear Zone. Access Ramps would be situated outside Clear Zones and outside majority of utility impacts.

| | | | |
|------------------------------------|-----------|------------------------------|---|
| Estimated Property Impacts: | 2 | Estimated Total Cost: | \$3,986,476 <i>4,218,812</i> |
| Estimated ROW Cost: | \$314,474 | Estimated CST Time: | 18 months |

Rationale: The avoidance of impacts to utilities on the northbound side of SR 21 and the ability to relocate the majority of utilities on the southbound side of SR 21 underground in the grassed buffer offsets the minimal cost of additional span length for the Pedestrian Bridge and additional ROW.

| | | | |
|---|-----|------------------------------|------|
| No-Build Alternative: | | | |
| Estimated Property Impacts: | 0 | Estimated Total Cost: | \$0 |
| Estimated ROW Cost: | \$0 | Estimated CST Time: | None |
| Rationale: This alternative was not chosen because it does not address the need to provide a safer alternative for pedestrians to cross SR 21 to/from Rice Creek School. | | | |

| | | | |
|---|-----------|------------------------------|-------------|
| Alternative 1: Construct a Pedestrian Bridge with Access Ramps that spans all lanes of SR 21 with the Access Ramps situated behind a two foot (2') grassed buffer and ten foot (10') sidewalk. | | | |
| Estimated Property Impacts: | 2 | Estimated Total Cost: | \$4,028,517 |
| Estimated ROW Cost: | \$279,274 | Estimated CST Time: | 18 months |
| Rationale: While this alternative proposes a shorter overall bridge span length and less ROW, additional protection in the form of guardrail would be required as well as utility relocations for all underground utilities. This alternative would not alleviate the requirement for the Georgia Power Transmission line to be raised / relocated. While the overall area of required ROW would be less, the number of impacted parcels would still be two (2). | | | |

Comments/Additional Information:

None

LIST OF ATTACHMENTS/SUPPORTING DATA

List and attach as appropriate to project. Please see PDP Appendix A for a more complete list of potential attachment.

1. Design-Build Recommendation to Chief Engineer
2. Concept Layout
3. Draft Bridge Layout
4. Cost Estimates
5. Project Meeting Minutes / Notes

A T T A C H M E N T 1

Design Build Recommendation to Chief Engineer

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE

FILE P.I. No. 0013549, Chatham Co
SR 21 @ CS 705/ Parkside Blvd
Pedestrian Bridge
Design-Build

OFFICE Innovative Delivery
DATE November 17, 2015

FROM  Darryl D. VanMeter, P.E., State Innovative Delivery Engineer

TO Meg B. Pirkle, P.E., Chief Engineer

SUBJECT REQUEST TO ADD PROJECT TO DESIGN-BUILD PROGRAM

The Department has identified an opportunity to expedite the delivery of the subject project utilizing the Design-Build delivery method. The Office of Innovative Delivery is requesting the addition of a SR 21 Pedestrian Bridge to GDOT's Design-Build Program.

Description of Project: Design and Construction of a pedestrian Bridge over SR 21 in Port Wentworth to also include sidewalk up to the nearest intersection of Rice Mill Road/ SR 21.

Preliminary Engineering Activities: In order to achieve a June 17, 2016 letting date, the Office of Innovative Delivery is looking at several available options to quickly develop the Design-Build Request for Proposals Package (RFP) that include:

- District 5 – Survey and property database
- State Utilities Office – SUE QL-B, obtain utility Memorandums of Understanding (MOU)
- Innovative Delivery Project Management Consultant (PMC)/ General Engineering Consultant (GEC) contracts – GEPA special studies, procurement support, as well as concept report, costing plans (20-30% plans) and ROW plans

Utilities: All Design-Build projects are designated as Public Interest Determination (PID), and utility owners are afforded opportunity to include costs in the Design-Build contract. Based on the attached preliminary cost estimate most items are reimbursable, and **based on the current preconstruction status report additional monies are needed for utility relocation costs.** GDOT intends to perform SUE QL-B to ensure that Design-Build teams have the most current information, and to support the development and execution of Design-Build Memorandums of Understanding (MOU) with each utility owner. In addition, **in order to achieve June 17, 2016 letting the project's utility phase must be programmed for FY 16.**

Right-of-Way: Additional ROW is required. The Innovative Delivery GEC consultant will develop the costing plans and ROW plans. It is currently anticipated that District 5 ROW will acquire ROW.

In addition, **in order to achieve June 17, 2016 letting the project's ROW phase must be programmed for FY 16.**

Environmental: Project will require a GEPA document. The Innovative Delivery PMC consultant will perform special studies and GDOT staff will develop the GEPA document.

Permitting: The awarded Design-Build Team will be responsible for preparing all applicable permits, and Stream Buffer Variance applications. It is anticipated that the awarded Design-Build Team will be responsible for acquiring the necessary mitigation credits.

MS4: MS4 screening will be performed by the Innovative Delivery GEC consultant as part of the development of the RFP package.

Geotechnical: GDOT is evaluating using an on-call drilling vendor to develop a representative number of boring logs for the pedestrian bridge.

Project Estimate: The current construction estimate is \$1.235M which is programmed for FY 18. The estimate will be revised as the RFP package is developed. **In order to achieve June 17, 2016 letting the project's construction phase must be programmed for FY 16.**

Proposed Schedule: The anticipated procurement and letting schedule for FY 16 is listed below:

- PNA – January 6, 2016
- Industry Forum – February 2016
- RFP advertisement – April 22, 2016
- Letting – June 17, 2016

Letting Responsibility: GDOT will let the project.

Design-Build Selection Method: GA Code 32-2-81 (2013) allows for the use of One Phase Low Bid, Two Phase Low Bid and Best Value. This project will utilize a **One Phase Low Bid** selection method.

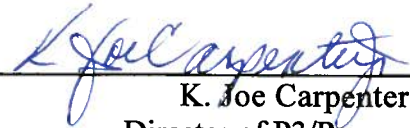
Shortlist: No shortlist. Since this project will utilize the One Phase Low Bid selection method, all Proposers meeting minimum qualifications as set forth in the RFP will be eligible.

Stipulated Fee: No stipulated fee will be offered since this project will utilize the One Phase Low Bid selection method. The Design-Build Manual table is listed as reference below.

| Design-Build Contract Value | Best Value | Two Phase Low Bid (w/ Shortlist) | Two Phase Low Bid (all qualified) | One Phase Low Bid |
|-----------------------------|----------------------------------|----------------------------------|-----------------------------------|-------------------|
| ≥\$50M | 0.2% Minimum 0.2% Recommended | 0.1% Minimum 0.2% Recommended | 0% Minimum 0.1% Recommended | No Stipulated Fee |
| <\$50M | 0.2% Minimum 0.4% Recommended | 0.1% Minimum 0.2% Recommended | 0% Minimum 0.1% Recommended | No Stipulated Fee |

Additional Comments: This project is a high priority. This office is leveraging all available resources (GDOT and consultant) to best organize the RFP, and to procure the Design-Build project. Additional discussions with GDOT SME's will ensue as the RFP package is being developed.

A Design-Build Suitability Assessment was performed and is available upon request. This office recommends your favorable approval of this request. Please contact Darryl VanMeter at 404-631-1703 should you need further information.

CONCUR: 
K. Joe Carpenter, P.E.,
Director of P3/Program Delivery

11/18/2015
Date

APPROVE: 
Meg B. Pirkle, P.E., Chief Engineer

11.18.15
Date


DVM:CAH:rtl

Attachments: Preconstruction Status Report
Project layout
Preliminary Utility Cost Estimate

cc: Director of Engineering, Glenn Bowman
Director of Permits & Operations, John Hibbard
Director of Construction, Jeff Baker
State Program Delivery Engineer, Albert Shelby
State Bridge Engineer, Bill DuVall
State Right-of-Way Administrator, Troy Byers
State Environmental Administrator, Hiral Patel
State Project Review Engineering, Lisa Myers
State Utilities Engineer, Lee Upkins
State Construction Engineer, Marc Mastronardi
State Materials Engineer, Chuck Hasty
State Transportation Office Engineer, Monica Flournoy
Transportation Engineer Administrator, Brent Story
State Roadway Design Engineer, Andy Casey
State Traffic Engineer, Andrew Heath
Program Control Administrator, Bobby Hilliard
State Transportation Planning Administrator, Cindy VanDyke
District 5 Engineer – Karon Ivery
District 5 Construction Engineer – Cory Knox
District 5 Preconstruction Engineer – Troy Pittman
District 5 Right of Way Engineer – Andy O'Quinn

Preconstruction Status Report

SR 21 @ CS 705/PARKSIDE BLVD IN PORT WENTWORTH

PI Number: 0013549
COUNTY: Chatham
LENGTH (MI): 0.4
PROJ NO:
PROJ MGR: Hoening, Andrew
AOHD INITIALS:
OFFICE: Innovative Prog. Delivery
CONSULTANT:
SPONSOR: GDOT

MPO: Savannah TMA
TIP #:
MODEL YR:
TYPE WORK: Pedestrian Crossings
CONCEPT: New Construction
PROG TYPE:
BOND PROJ:
DESIGN FIRM:

PRIORITY CD: 5
DOT DIST: 1
CONG. DIST: 1
BIKE: N
MEASURE:
SUFF:

BASELINE LET DAT
SCHED LET DATE:
LIGHTING TYPE: None
ENV DOC TYPE: GEPA
ENV CONSULTANT

MGMT LET DATE:
MGMT ROW DATE:
WHO LETS?
LET WITH:
GDOT Let

| BASE START | BASE FINISH | TASKS | START DATE | FINISH DATE | ACTUAL START | ACTUAL FINISH | % |
|------------|-------------|---|------------|-------------|--------------|---------------|---|
| | | GEPA B | | | | | |
| | | TO BE DETERMINED | | | | | |
| | | SAVANNAH TIP AMENDMENT 8-26-2015 | | | | | |
| | | STATE FUNDED CAPITAL PROGRAM/PE 1625 9-2015 | | | | | |

| Phase | Approved | Proposed | Cost | Fund | Status | Date Auth |
|-------|----------|----------|----------------|-------|------------|-----------|
| PE | 2016 | 2016 | \$114,856.00 | HB170 | AUTHORIZED | 9/15/15 |
| ROW | 2017 | 2017 | \$187,000.00 | HB170 | PRECST | |
| CST | 2018 | 2018 | \$1,235,696.00 | HB170 | PRECST | |
| UTL | 2018 | 2018 | \$200,000.00 | HB170 | PRECST | |

COST ESTIMATES

| Phase | Approved | Proposed | Cost | Fund |
|-------|----------------|----------|------|------|
| PE | \$114,856.00 | | | |
| ROW | \$187,000.00 | | | |
| CST | \$1,235,696.00 | | | |
| UTL | \$200,000.00 | | | |

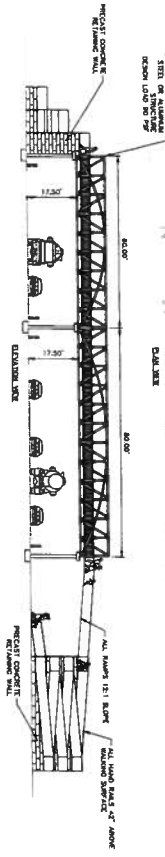
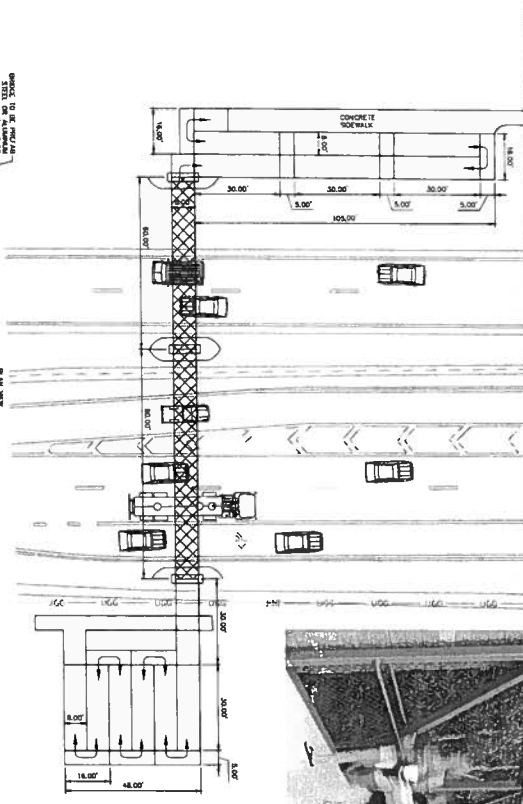
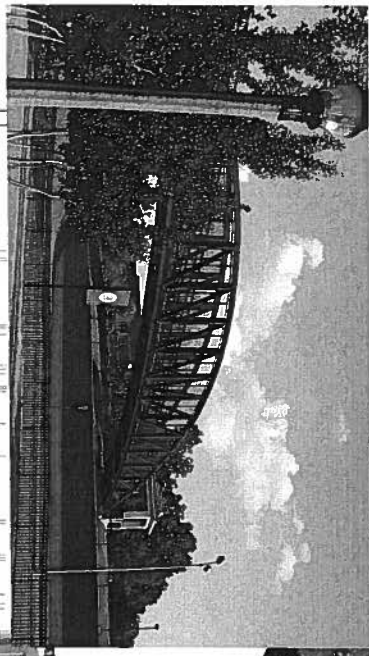
STIP AMOUNTS

| Activity | Cost | Fund |
|----------|----------------|-------|
| PE | \$114,856.00 | HB170 |
| ROW | \$187,000.00 | HB170 |
| CST | \$1,235,696.00 | HB170 |
| UTL | \$200,000.00 | HB170 |

District Comments

STATE FUNDS ONLY
pedestrian bridge in Chatham. Commissioner priority. (9-9-15 AVS)

| | | | | |
|----------------------|------------------------------------|---------------------|-------------------------|------------------|
| Pre Parcel CT | Total Parcel in ROW System: | Cond Filed: | Acquired by: DOT | DEEDS CT: |
| Under Review | Options Pending: | Relocations: | Acquisition MGR: | |
| Released: | Condemnations - Pend: | Acquired: | ROW Cert Date: | |



C-01

| NO. | DESCRIPTION |
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**PEDESTRIAN CROSSWALK
SR 21/AUGUSTA RD
PORT WENTWORTH**

PROJECT NAME
CONCEPT PLAN

T. R. LONG ENGINEERING, P.C.


T. R. Long Engineering, P.C.
114 North Commerce Street
Hinesville, Georgia 31313
Office (912) 368-5664
Fax (912) 368-7206

DATE PLOTTED: 03/09/2012 11:08 AM
PLOTTER: HP DesignJet 2480
SCALE: 1/8" = 1'-0"

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE PI No. 0013549 Chatham County DATE: 11-10-2015
SR 21 @ CS 705/Parkside Blvd in Port Wentworth

FROM 
Dallyn Rozier, District Utilities Engineer

TO Andrew Hoenig, Project Manager

SUBJECT PRELIMINARY UTILITY COST (ESTIMATE)

As requested by your office, we are furnishing you with an updated Preliminary Utility Cost Estimate of each utility with facilities potentially located within the above project limits.

| Facility Owner | Non-Reimbursable | Reimbursable | Comments |
|----------------------------|------------------|--------------|----------|
| AT&T | \$32,000.00 | | |
| Atlanta Gas Light | | \$11,500.00 | |
| Comcast | | \$7,500.00 | |
| City of Port Wentworth | | \$15,200.00 | |
| Georgia Power Distribution | | \$32,500.00 | |
| Georgia Power Transmission | | \$800,000.00 | |
| Hargray Communications | | \$8,750.00 | |
| | | | |
| | | | |
| Totals | \$32,000.00 | \$875,450.00 | |
| Total Reimbursement | | \$875,450.00 | |

Note: Due to Design Build Policy, project automatically is a PID.

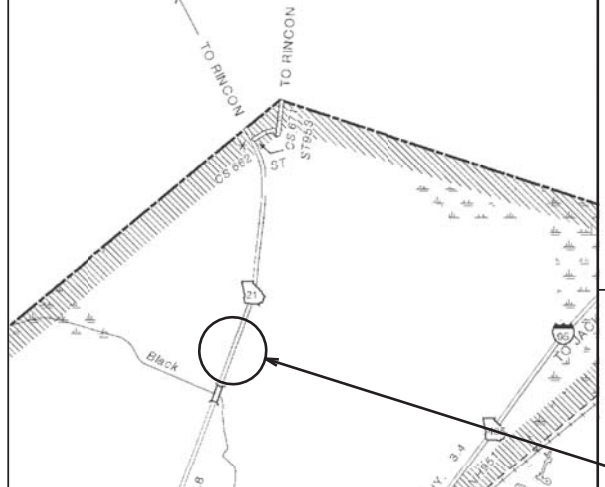
CC: Lee Upkins, State Utilities Engineer
Kerry Gore, Assistant State Utilities Engineer
Yulanda Pride-Foster, Utilities Preconstruction Engineer
Merishia Robinson, Utilities Preconstruction Engineer
Vahid Munshi, Management Specialist
District Office File
Utilities Office File

A T T A C H M E N T 2
Concept Designs

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

PLAN AND PROFILE OF PROPOSED SR 21 @ CS 705/PARKSIDE BLVD PEDESTRIAN BRIDGE

FEDERAL ROUTE * N/A
STATE ROUTE * 21
P.I. NO. 0013549



LOCATION SKETCH

DESIGN DATA:
 TRAFFIC A.D.T.: N/A
 TRAFFIC A.D.T.: N/A
 TRAFFIC D.H.V.: N/A
 DIRECTIONAL DIST: N/A
 % TRUCKS: N/A
 24 HR. TRUCKS %: N/A
 SPEED DESIGN: 55 mph

LOCATION & DESIGN APPROVAL DATE:

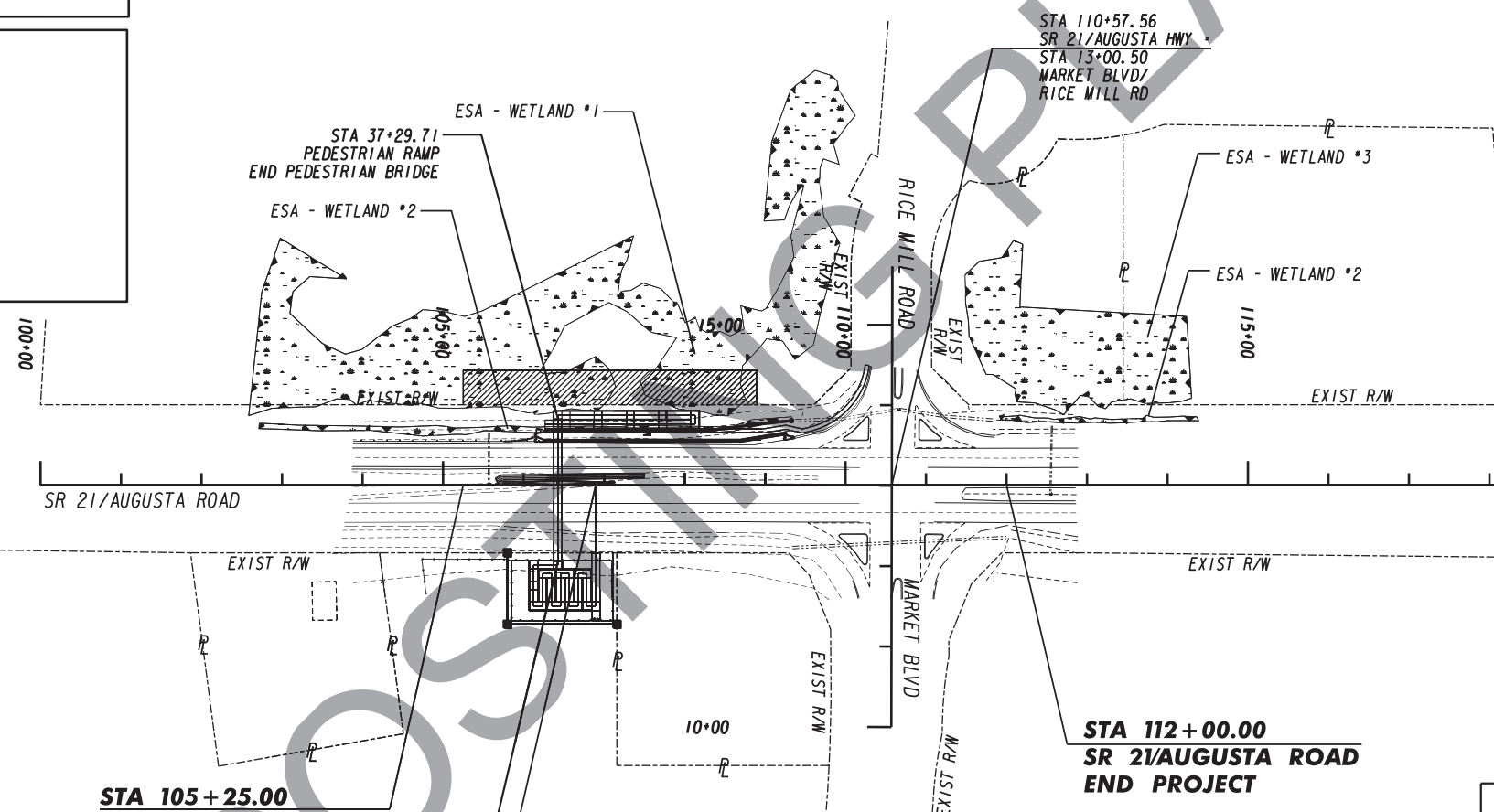
FUNCTIONAL CLASS:

THIS PROJECT IS 100% IN CHATHAM COUNTY AND IS 100% IN CONG. DIST. NO. 5

PROJECT DESIGNATION:
DESIGNED IN ENGLISH UNITS.

THIS PROJECT HAS BEEN PREPARED USING THE HORIZONTAL GEORGIA COORDINATE SYSTEM OF 1984 (NAD 1983/94 WEST ZONE, AND THE NORTH AMERICAN VERTICAL DATUM (NAVD) OF 1988.

THE DATA, TOGETHER WITH ALL OTHER INFORMATION SHOWN ON THESE PLANS OR IN ANYWAY INDICATED THEREBY, WHETHER BY DRAWINGS OR NOTES, OR IN ANY OTHER MANNER, ARE BASED UPON FIELD INVESTIGATIONS AND ARE BELIEVED TO BE INDICATIVE OF ACTUAL CONDITIONS. HOWEVER, THE SAME ARE SHOWN AS INFORMATION ONLY, ARE NOT GUARANTEED, AND DO NOT BIND THE DEPARTMENT OF TRANSPORTATION IN ANY WAY. THE ATTENTION OF BIDDER IS SPECIFICALLY DIRECTED TO SUBSECTIONS 102.04, 102.05, AND 104.03 OF THE SPECIFICATIONS.



NOTE:
 ALL REFERENCES IN THIS DOCUMENT, WHICH INCLUDES ALL PAPERS, WRITINGS, DOCUMENTS, DRAWINGS, OR PHOTOGRAPHS USED, OR TO BE USED IN CONNECTION WITH THIS DOCUMENT TO "STATE HIGHWAY DEPARTMENT OF GEORGIA"; "STATE HIGHWAY DEPARTMENT"; "GEORGIA STATE HIGHWAY DEPARTMENT"; "HIGHWAY DEPARTMENT"; OR "DEPARTMENT" WHEN THE CONTEXT THEREOF MEANS THE STATE HIGHWAY DEPARTMENT OF GEORGIA, AND SHALL BE DEEMED TO MEAN THE DEPARTMENT OF TRANSPORTATION.

ARCADIS Design & Consultancy for natural and built assets

2410 Paces Ferry Road, Suite 400
 Atlanta, Georgia 30339
 Tel: 770/431-8666
 Fax: 770/435-2666

PREPARED BY: _____
 DESIGN

RECOMMENDED FOR SUBMISSION BY: _____
 DESIGN

SUBMITTED BY: _____
 STATE DESIGN ENGINEER

| LENGTH OF PROJECT | COUNTY No. 51 Project No. 0013549 |
|--------------------------|---|
| MILES | |
| NET LENGTH OF ROADWAY | 0.089 |
| NET LENGTH OF BRIDGES | 0.000 |
| NET LENGTH OF PROJECT | 0.089 |
| NET LENGTH OF EXCEPTIONS | 0.000 |
| GROSS LENGTH OF PROJECT | 0.089 |

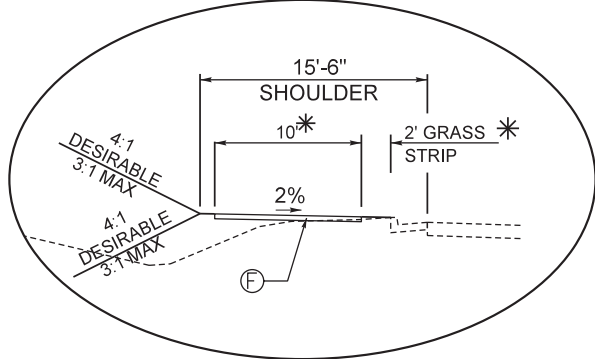


Know what's below.
Call before you dig.

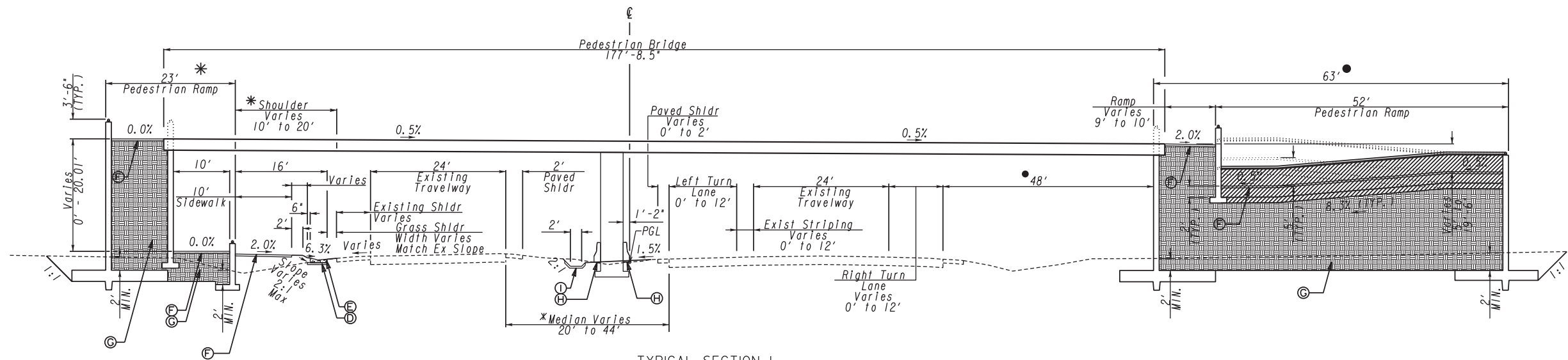
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| PLANS COMPLETED | -- |
| REVISIONS | |
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DRAWING No.
01-001

SHOULDER DETAIL LT



* STA 109+46.70 TO STA 110+29.46 LT



TYPICAL SECTION I
SR 21/AUGUSTA HWY

STA 104+98.88 TO STA 110+29.46

DESCRIPTION OF PROPOSED IMPROVEMENTS (LT)

- * INSTALL 10' CONC SIDEWALK W/3' BUFFER STA 106+36.71 TO STA 109+27.30 LT
- TRANSITION 10' CONC SIDEWALK W/3' BUFFER TO 10' CONC SIDEWALK W/2' BUFFER STA 109+27.30 TO 109+46.70
- INSTALL 10' CONC SIDEWALK W/2' BUFFER STA 109+46.70 TO STA 110+29.46 (SEE SHOULDER DETAIL LT)
- INSTALL CONC "V" GUTTER STA 106+26.71 TO STA 109+26.85
- INSTALL 10' PEDESTRIAN RAMP STA 106+36.71 TO STA 108+18.71 (SEE SHOULDER DETAIL 1 FOR WALKWAY TO RAMP)

DESCRIPTION OF PROPOSED IMPROVEMENTS (MEDIAN)

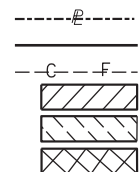
- * INSTALL TYPE P ATTENUATOR STA 105+72.38 TO STA 105+92.88 STA 106+92.54 TO STA 107+13.04
- INSTALL TYPE 7C SIDE BARRIER STA 105+92.88 TO STA 106+92.54
- INSTALL 4' PAVED DITCH STA 105+65.00 TO STA 107+50.00

DESCRIPTION OF PROPOSED IMPROVEMENTS (RT)

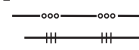
- INSTALL 10' PEDESTRIAN RAMP STA 106+06.71 TO STA 106+84.71 (SEE SHOULDER DETAIL 2 FOR WALKWAY TO RAMP)

- Ⓐ GR AGGR BASE CRS, 4 INCH, INCL MATL
- Ⓑ CONCRETE V GUTTER
- Ⓒ CONC SIDEWALK/WALKWAY/RAMP, 4 IN
- Ⓓ MSE WALL BACKFILL MATERIAL
- Ⓔ CONCRETE SIDE BARRIER, TYPE 7C
- Ⓕ PLAIN CONC DITCH PAVING, 4 IN

PROPERTY AND EXISTING R/W LINE
REQUIRED R/W LINE
CONSTRUCTION LIMITS
EASEMENT FOR CONSTR
& MAINTENANCE OF SLOPES
EASEMENT FOR CONSTR OF SLOPES
EASEMENT FOR CONSTR OF DRIVES



BEGIN LIMIT OF ACCESS.....BLA
END LIMIT OF ACCESS.....ELA
LIMIT OF ACCESS
REQ'D R/W & LIMIT OF ACCESS



N. T. S.

REVISION DATES

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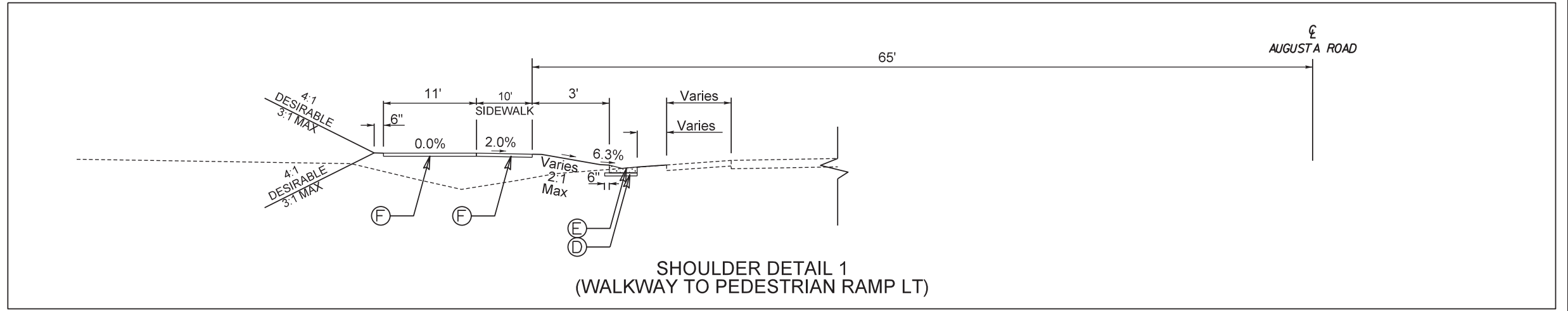
STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION

OFFICE:

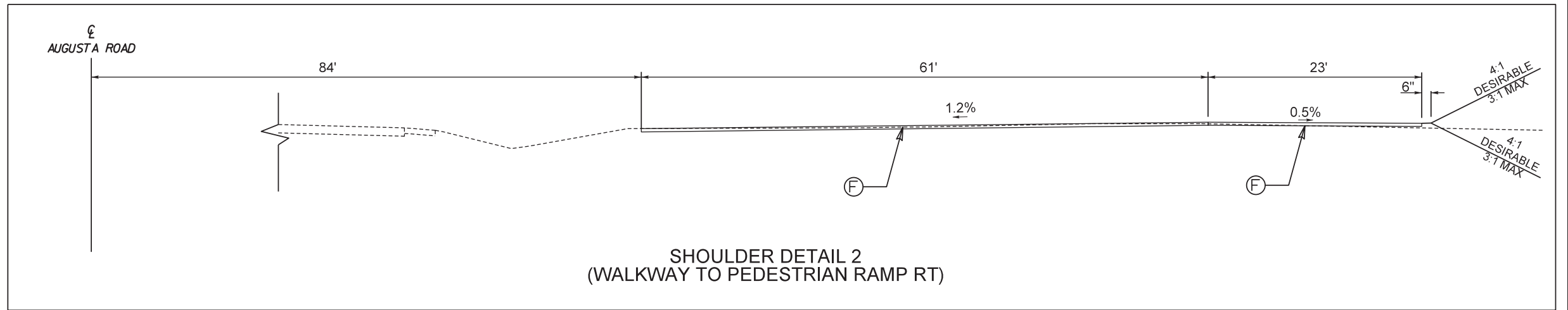
TYPICAL SECTIONS

SR 21 @ CS 705/PARKSIDE BLVD
PEDESTRIAN BRIDGE

DRAWING No.
05-001



* STA 106+26.71 TO STA 106+36.71 LT



● STA 106+84.71 TO STA 106+94.71 RT

- Ⓧ GR AGGR BASE CRS, 4 INCH, INCL MATL
- Ⓧ CONCRETE V GUTTER
- Ⓧ CONC SIDEWALK/WALKWAY/RAMP, 4 IN
- Ⓧ MSE WALL BACKFILL MATERIAL
- Ⓧ CONCRETE SIDE BARRIER, TYPE 7C
- Ⓧ PLAIN CONC DITCH PAVING, 4 IN

SHEET 18
 SHEET 17
 SHEET 16
 SHEET 15
 SHEET 14
 SHEET 13
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 SHEET 11
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REVISION DATES

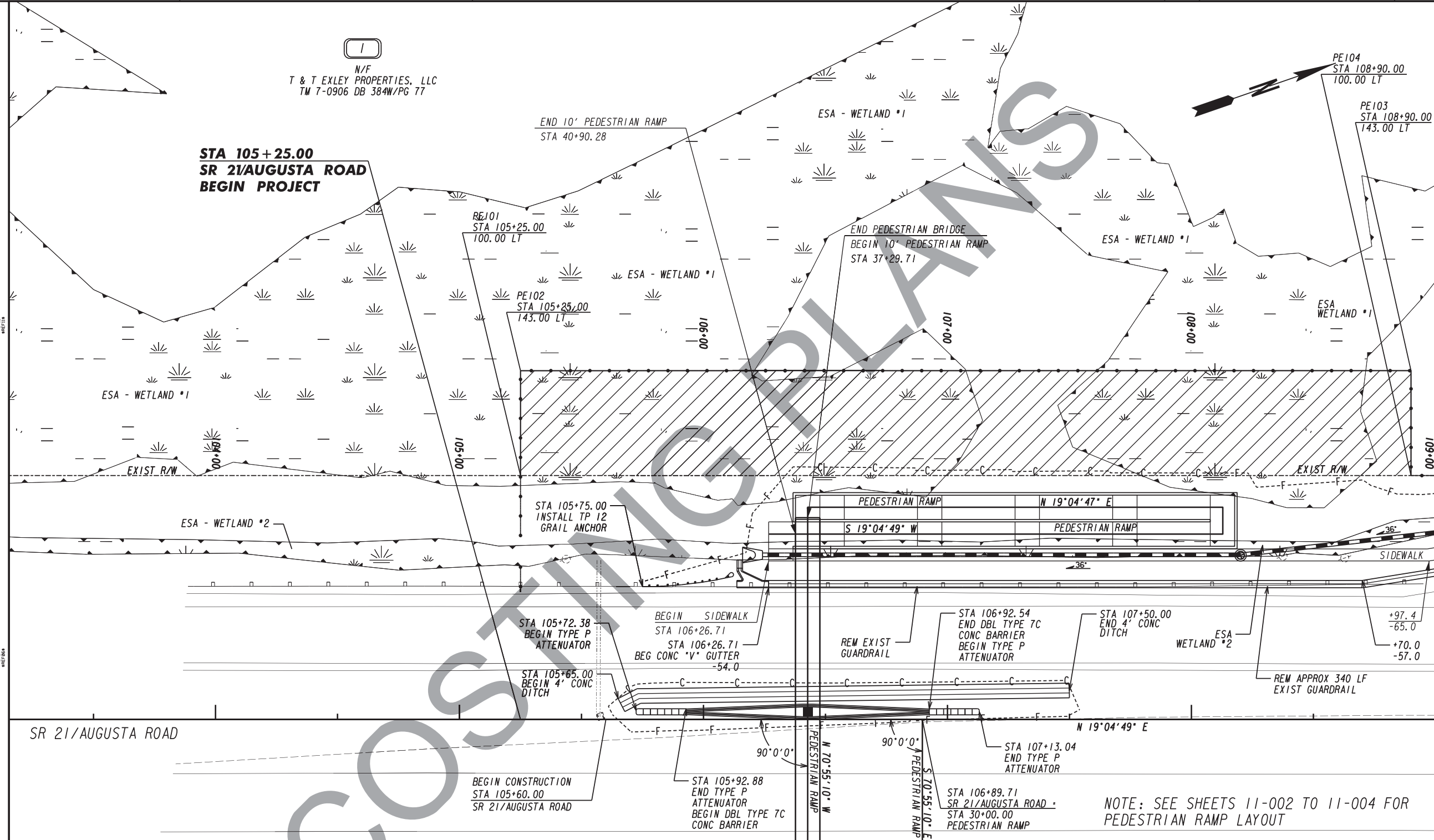
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STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION

OFFICE: **TYPICAL SECTIONS**

SR 21 @ CS 705/PARKSIDE BLVD
PEDESTRIAN BRIDGE

DRAWING No. **05-002**



MATCH LINE STA 109+00 - SEE DWG 13-003

MATCH LINE STA 30+50 - SEE DWG 13-002

PROPERTY AND EXISTING R/W LINE
 REQUIRED R/W LINE
 CONSTRUCTION LIMITS
 EASEMENT FOR CONSTR
 & MAINTENANCE OF SLOPES
 EASEMENT FOR CONSTR OF SLOPES
 EASEMENT FOR CONSTR OF DRIVES

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 LIMIT OF ACCESS
 REQ'D R/W & LIMIT OF ACCESS



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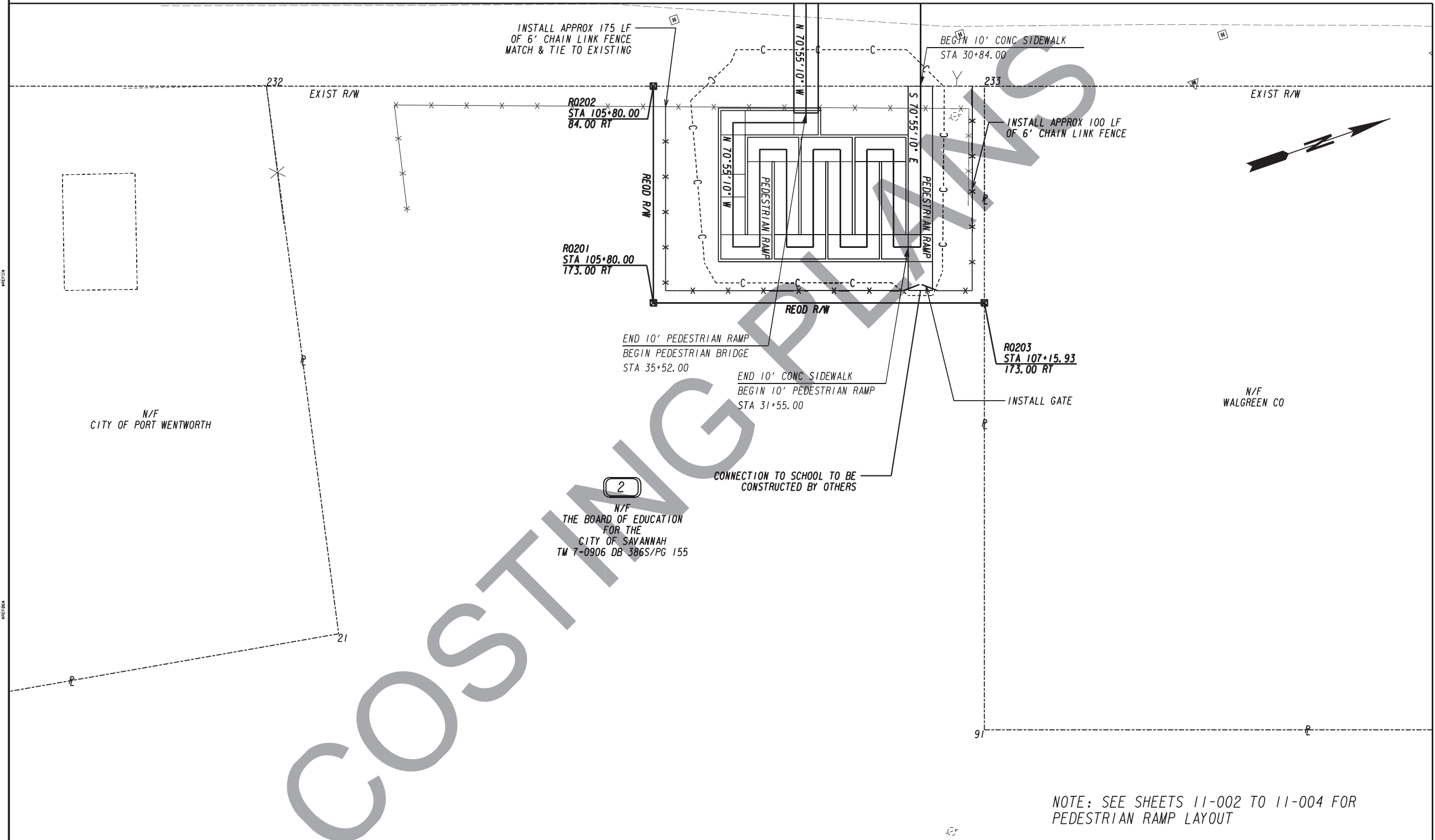
STATE OF GEORGIA
 DEPARTMENT OF TRANSPORTATION

OFFICE: **MAINLINE PLAN**

SR 21 @ CS 705/PARKSIDE BLVD
 PEDESTRIAN BRIDGE

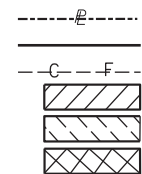
DRAWING No.
13-001

MATCH LINE STA 109+00 - SEE DWG 13-001



NOTE: SEE SHEETS 11-002 TO 11-004 FOR PEDESTRIAN RAMP LAYOUT

PROPERTY AND EXISTING R/W LINE
 REQUIRED R/W LINE
 CONSTRUCTION LIMITS
 EASEMENT FOR CONSTR
 & MAINTENANCE OF SLOPES
 EASEMENT FOR CONSTR OF SLOPES
 EASEMENT FOR CONSTR OF DRIVES



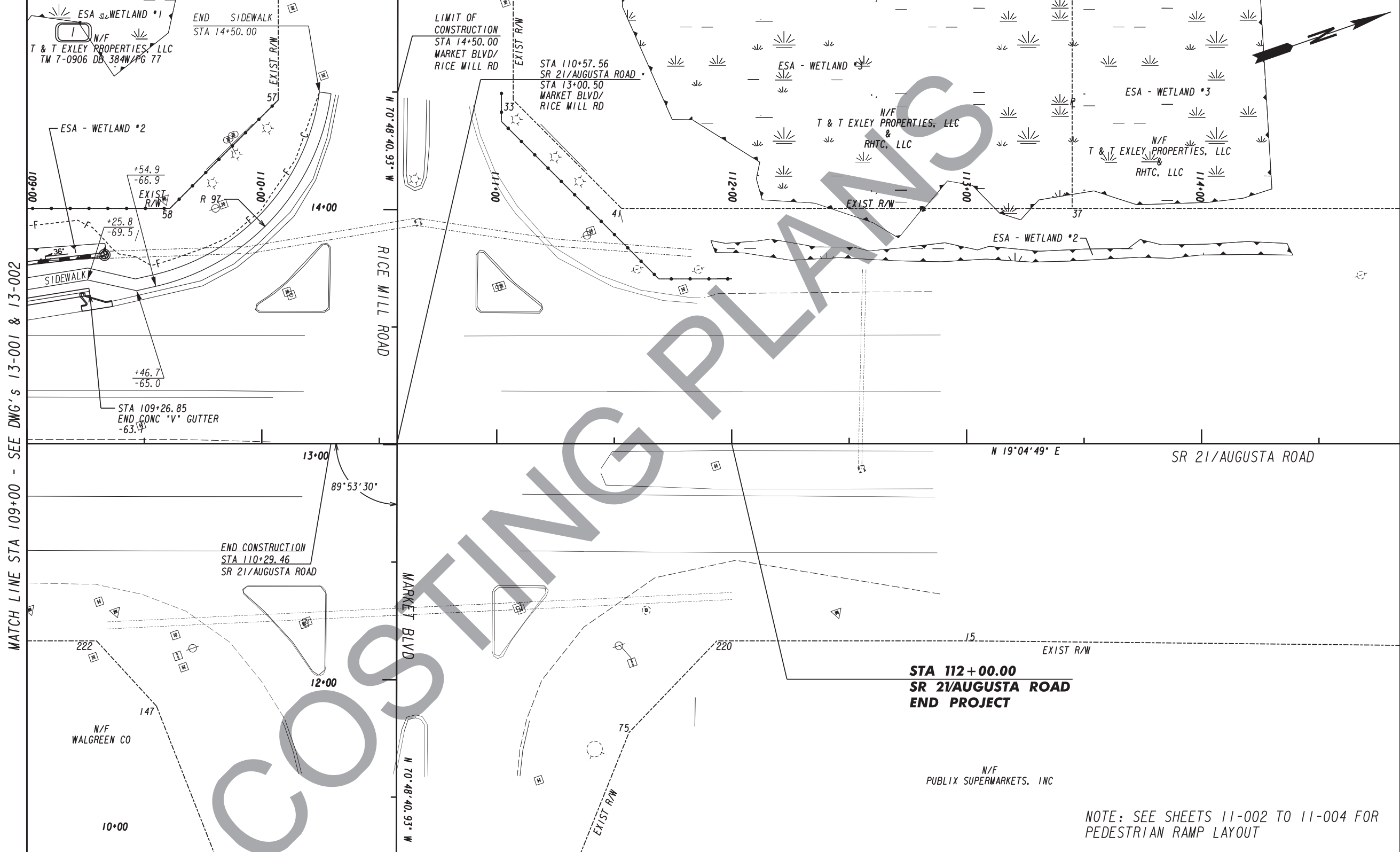
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| REVISION DATES | |
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STATE OF GEORGIA
 DEPARTMENT OF TRANSPORTATION
 OFFICE: **MAINLINE PLAN**
 SR 21 @ CS 705/PARKSIDE BLVD
 PEDESTRIAN BRIDGE
 DRAWING No. **13-002**

MATCH LINE STA 109+00 - SEE DWG 13-003

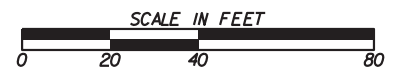


MATCH LINE STA 109+00 - SEE DWG'S 13-001 & 13-002

NOTE: SEE SHEETS 11-002 TO 11-004 FOR PEDESTRIAN RAMP LAYOUT

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| PROPERTY AND EXISTING R/W LINE | --- |
| REQUIRED R/W LINE | --- |
| CONSTRUCTION LIMITS | --- |
| EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES | ▨ |
| EASEMENT FOR CONSTR OF SLOPES | ▩ |
| EASEMENT FOR CONSTR OF DRIVES | ▧ |

| | |
|-------------------------------|-----|
| BEGIN LIMIT OF ACCESS.....BLA | --- |
| END LIMIT OF ACCESS.....ELA | --- |
| LIMIT OF ACCESS | --- |
| REQ'D R/W & LIMIT OF ACCESS | --- |



| REVISION DATES |
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STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION

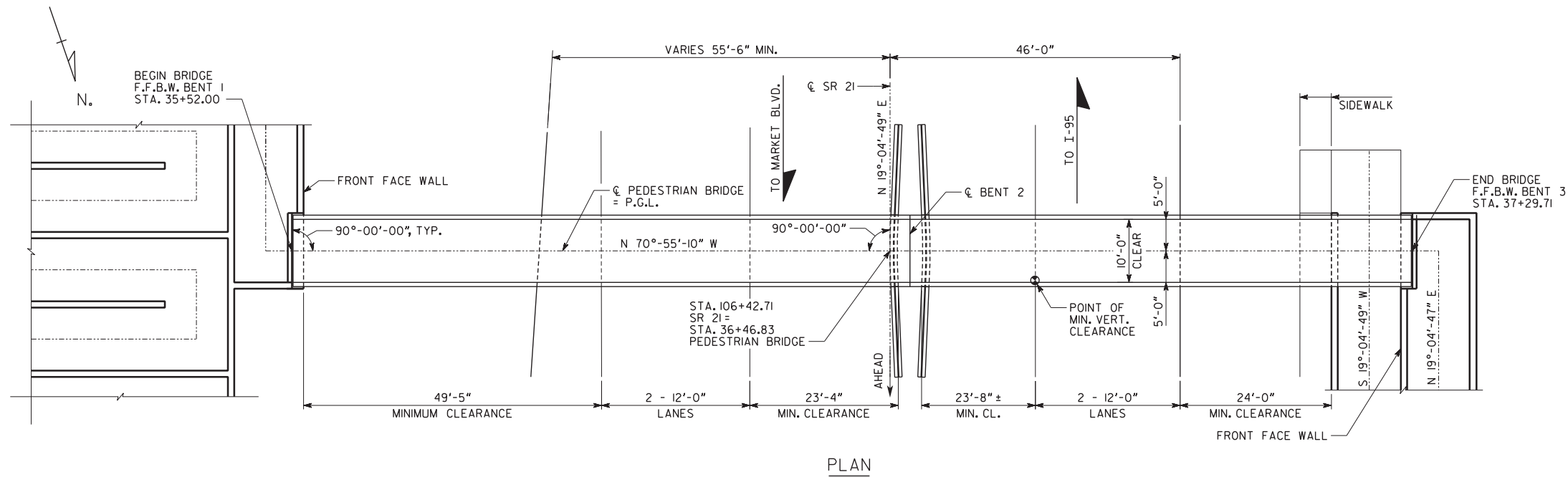
OFFICE: **MAINLINE PLAN**

SR 21 @ CS 705/PARKSIDE BLVD
PEDESTRIAN BRIDGE

DRAWING No. **13-003**

A T T A C H M E N T 3
Draft Bridge Layout

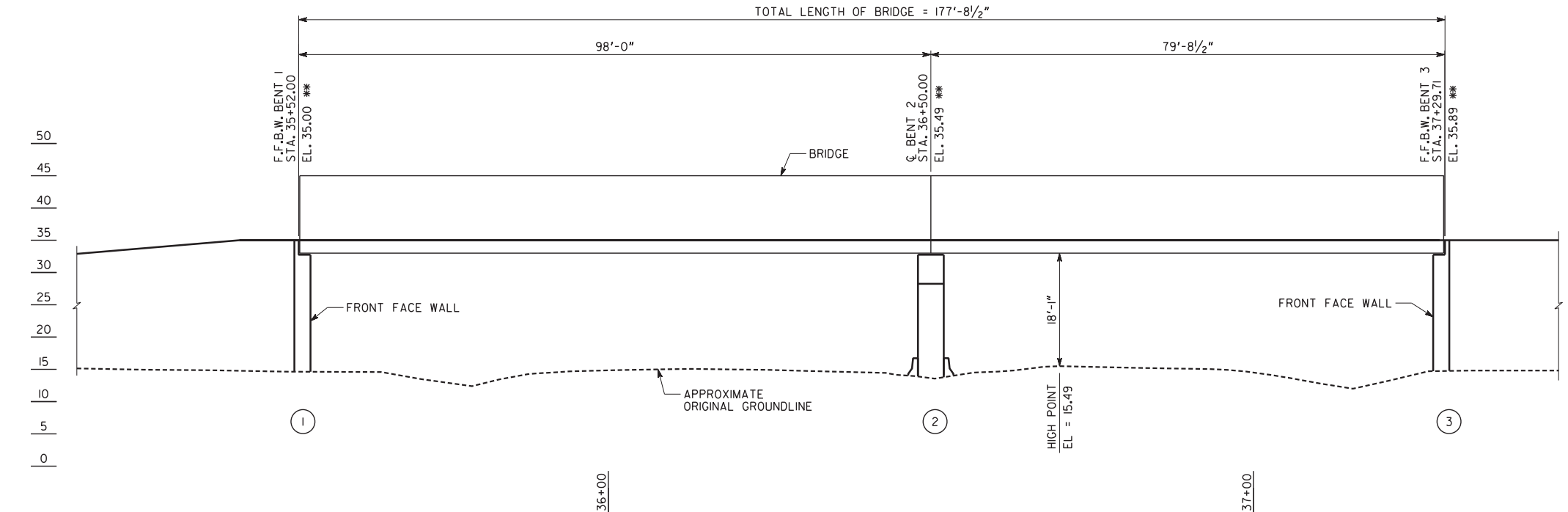
USER: ereize



PVI STA. 35+52.00
PVI EL. 35.00

+0.5000%

PROPOSED GRADE DATA
PEDESTRIAN BRIDGE



PROJECT P.I. NO. 0013549

BRIDGE NO. 1

- BRIDGE CONSISTS OF**
- 1 - 98'-0" SPAN ----- SPECIAL DESIGN
 - 1 - 79'-8 1/2" SPAN ----- SPECIAL DESIGN
 - 2 - END BENTS ----- SPECIAL DESIGN
 - 1 - INTERMEDIATE BENT ----- SPECIAL DESIGN

DESIGN DATA

SPECIFICATIONS ----- AASHTO LRFD 7TH EDITION, 2014

DESIGN VEHICLE LIVE LOAD ----- HL-93

FUTURE PAVING ALLOWANCE ----- 30 LBS PER SQ FT

- NOTES:**
1. ALL BENTS ARE PARALLEL.
 2. ** STATIONS AND ELEVATIONS ARE ALONG PROFILE GRADE LINE AT THE INTERSECTION OF PROFILE GRADE LINE AND F.F.B.W. OR C BENT.
 3. F.F.B.W. = FRONT FACE BACK WALL.

ARCADIS Design & Consultancy for natural and built assets

240 PACES FERRY ROAD, SUITE 400
ATLANTA, GEORGIA 30339
TEL: 770/431-8666 FAX: 770/435-2666

GEORGIA
DEPARTMENT OF TRANSPORTATION
ENGINEERING DIVISION-OFFICE OF BRIDGES AND STRUCTURES

COSTING LAYOUT
PEDESTRIAN BRIDGE OVER SR 21
CHATHAM COUNTY 0013549

SCALE: 1" = 10' MARCH 2016

DRAWING NO. 35-0001
BRIDGE SHEET 1 OF 1

| REVISIONS | DATE |
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|--------------|--------------|------------------|
| DESIGNED KAK | CHECKED | REVIEWED SKG/DLC |
| DRAWN JJA | DESIGN GROUP | APPROVED WMD |

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TIME: 9:39:11 AM

DATE: 5/6/2016

A T T A C H M E N T 4
Cost Estimate

DATE : 04/13/2016
 PAGE : 1

JOB DETAIL ESTIMATE

JOB NUMBER : 0013549 - DB SPEC YEAR: 13
 DESCRIPTION: SR 21 Pedestrian Bridge - DB

ITEMS FOR JOB 000013549 - DB

| LINE | ITEM | ALT | UNITS | DESCRIPTION | QUANTITY | PRICE | AMOUNT |
|------------------------------|----------|-----|-------|-----------------------|----------|------------|------------|
| 0001 | 999-2010 | | LS | DESIGN COMPLETE | 1.000 | 190701.00 | 190701.00 |
| 0002 | 999-2015 | | LS | CONSTRUCTION COMPLETE | 1.000 | 3168079.00 | 3356759.00 |
| ITEM TOTAL | | | | | | | 3547460.00 |
| INFLATED ITEM TOTAL | | | | | | | 3547460.00 |
| TOTALS FOR JOB 0013549 - DB | | | | | | | |
| ESTIMATED COST: | | | | | | | 3547460.00 |
| DESIGN CONTINGENCY (15.0): | | | | | | | 503514.00 |
| E&I (5.0): | | | | | | | 167838.00 |
| ESTIMATED TOTAL: | | | | | | | 4218812.00 |

A T T A C H M E N T 5
Meeting Minutes

DESIGN/BUILD REQUEST FOR PROPOSAL REPORT

PI No.: 0013549, Chatham County

Pedestrian Bridge – SR 21 at CS 705 Parkside Blvd in Port Wentworth

INSPECTION DATE: 3/31/16
REPORT DATE: 4/1/16
RESPONSE ACCEPTED DATE:

This inspection was requested by Darryl D. VanMeter, P.E., State Innovative Delivery Engineer. The Project Manager is Andrew Hoenig.

The Request for Proposals was prepared by Arcadis U.S., Inc.

The report was prepared by John Kopotic, Design Review Specialist 2, and accepted by Lisa L. Myers, State Project Review Engineer, Office of Engineering Services.

The GEPA document is pending approval.

The Project Manager is reminded that as of the RFP Review of 3/31/16, GEPA evaluations were still underway. The Project Manager is advised that the information contained in this report was based on incomplete environmental condition studies and possible commitments as were known at the time. The RFP Review inspection team's review was limited to these incomplete environmental condition studies and possible commitments. Verify that all commitments from the completed approved GEPA document have been incorporated into the plan set and Special Provisions.

This report is being distributed via E-mail. Final plan preparation can now begin.

The Inspection Plans were reviewed in Port Wentworth City Hall Conference Room prior to the site inspection.

All comments marked with an arrow symbol (⇒) should be addressed with a written response by the Project Manager.

Projects Let to construction after July 1, 2013 will require the use of the 2011 AASHTO "GREEN BOOK", GDOT Design Manual, 2011 Roadside Design Guide, 2012 Guide for the Development of Bicycle Facilities and the 2001 Georgia Standard Specifications. Please revise all notes that make reference to previous GDOT Design Manuals and Specifications. Any substandard features that cannot be complied with due to project restraints will require the submission of a design exception/variance to the State Design Policy and Support Engineer.

PROJECT DESCRIPTION

The proposed project will install a pedestrian bridge over SR 21 near the intersection of Rice Mill Road / Market Boulevard at SR 21. The project will install a pedestrian bridge across SR 21 and will span all existing lanes of SR 21. Access ramps meeting ADA standards will be installed at both termini of the pedestrian bridge. The project has a total project length of 0.12 miles. The project is scheduled for advertisement on April 22, 2016.

DESIGN DATA – N/A

ENVIRONMENTAL

ENVIRONMENTAL COMMITMENTS:

GENERAL ENVIRONMENTAL COMMENTS:

⇒ The Environmental Resources Impact Table ([Environmental Resources Impact Table](#)) will be coordinated with the project's Environmental Commitments Table and other plan notes to illustrate the restrictions associated with various environmental resources. The responsibility for inserting the table into the plans will rest with the

designer, and its content and accuracy will be confirmed by the NEPA analyst who is responsible for environmentally certifying the project. The table shall be included in the General Notes section of all plans beginning with projects that have field plan reviews scheduled for September 1, 2010, and later and/or projects that are scheduled to be let in November 2010 or later. All Environmentally Sensitive Area (ESA) flags on plan sheets shall be changed to read "ESA - See Environmental Resources Impact Table in General Notes for construction restrictions." The ESA notes shall be placed on ALL plan sheets where an ESA exists.

The GEPA document will be approved after the project is let, but prior to the Design-Build team beginning construction activities.

Jeffrey Garnett's primary comment as it concerns Ecology pertains to Section 4.2.2 in Volume 3 of the Draft RFP. He believes a statement should be added that the DB Team should be aware of permit expiration dates and shall assume all schedule risk when applying for necessary permits. He says this because:

1. It seems likely that this project will involve wetland impacts, and
GDOT OID Response: Yes. Wetland impacts will result from the construction of the western Access Ramp.
2. Expiration dates for Nationwide and Regional 404 permits are around the corner. The current NWP's expire 3/18/2017, and both RP 1 and RP 96 expire around 10/2018. With NWP's construction must begin before the expiration date, but projects that break ground prior to expiration have a year to complete construction. With RPs, however, there is no grace period (i.e. work must be complete by expiration date). According to the draft RFP timeline, the contractor has until winter of 2018 to complete preliminary design/studies and construction (assuming NTP 1 is issued this summer), which is in compliance with either permit type; however, we all know things rarely stay on schedule. Therefore, I believe the DB Team should be cognizant of the permit deadlines and risks involved.
GDOT OID Response: We are aware of the pending permit expirations. Verbiage will be added to Volume 2 of the Request for Proposals (RFP) to address this.

RIGHT OF WAY

NUMBER OF PARCELS: 2

APPRAISED: 0

ACQUIRED DEEDS: 0

ACQUISITION BY: 0

TYPE ACCESS CONTROL: By permit

GENERAL RIGHT OF WAY COMMENTS: None

DESIGN EXCEPTIONS

DESIGN EXCEPTIONS REQUIRED: To be Determined by the Design Build Team

DESIGN EXCEPTIONS REQUESTED: None

DESIGN EXCEPTIONS APPROVED: None

DESIGN VARIANCES

DESIGN VARIANCES REQUIRED: To be Determined by the Design Build Team

DESIGN VARIANCES REQUESTED: None

DESIGN VARIANCES APPROVED: None

SPECIAL PROVISIONS

PROJECT SPECIFIC SPECIAL PROVISIONS FURNISHED FOR THE INSPECTION:

Section 534 – Pedestrian Overpass Bridge

⇒ ADDITIONAL PROJECT SPECIFIC SPECIAL PROVISIONS REQUIRED:

Utility Conflicts – All projects

Section 107.21 – Contractor's Worksite Utility Coordination Supervisor (WUCS) *(included in ALL projects)*

Section 647 - Traffic Signal Installation

Section 660 – Sanitary Sewer System

Section 664 – Electrical Distribution Systems

Section 665 – Gas Distribution System

Section 670 – Water Distribution System

Section 925—Traffic Signal Equipment

Section 950 – Telecommunication Facilities

Section 951 – Cable Systems

Section 663 - Electrical Transmission Systems

GDOT OID Response: All required Special Provisions will be added to the RFP.

GENERAL SPECIAL PROVISION COMMENTS:

Contracts Administration should not include the Special Provision adding the Fine Grader due to this being a Pedestrian Overpass Bridge with no pavement.

Restrictions to traffic interruptions are recommended.

SR 21

A minimum of two (2) travel lanes in each direction shall remain open to the travelling public at all times. An exception to the minimum number of lanes open may be allowed for the center lane construction subject to approval by GDOT.

Lane and shoulder closures will NOT be allowed between the following hours:

| <u>Day</u> | <u>Southbound Lanes</u> | <u>Northbound Lanes</u> |
|-----------------------|------------------------------|------------------------------|
| Monday through Friday | 5:00 a.m. to 8:00 p.m. daily | 5:00 a.m. to 9:00 p.m. daily |
| Saturday | 10:00 a.m. to 8:00 p.m. | 10:00 a.m. to 8:00 p.m. |
| Sunday | 10:00 a.m. to 8:00 p.m. | 10:00 a.m. to 8:00 p.m. |

All Other Roads and Streets

A minimum of one (1) travel lane in each direction shall remain open for all other roads and streets. No side streets will be allowed to be closed unless approved by GDOT or the Governmental Entity having jurisdiction of the cross street. The DB Team shall not install lane closures, perform flagging, or move equipment on the travel way on any day between the hours of 5:30 a.m. to 9:30 a.m. and from 4 p.m. and 7 p.m. The DB Team shall coordinate with Local Government Agency having jurisdiction to determine acceptable times for closure to occur.

⇒ Please submit the Exhibit 18 of the DB Agreement and Sections 18.2 and 18.3 of Volume 2 – Technical Provisions to the Office of Construction for their review. This should be done after the Request for Proposal Review, but prior to the assembly of the Final Plan Documents.

GDOT OID Response: The Final version of Exhibit 18 and Volume 2, Section 18 will be submitted to GDOT Office of Construction and District 5 Construction for review and comment.

VALUE ENGINEERING - N/A

Total Project Programmed Cost: \$1,737,552.00

There are no design drawings submitted for the Request for Proposals Review. Only a draft contract document has been provided. All drawing section headings have been removed. All design drawings required will be provided by the design build contract winner at the final review.

UTILITY INFORMATION

| | |
|-------------|--|
| Electrical: | Georgia Power – Distribution Georgia Power - Transmission |
| Gas: | Atlanta Gas Light Co. |
| Sewer: | City of Port Wentworth |
| Water: | City of Port Wentworth |
| Telephone: | ATT Hargray Telecom |
| Railroad: | None |
| Cable TV: | Comcast |
| Other: | None |

Utility location was performed by SUE, Level B

In accordance with TOPPS Policy 6863-12, responsibility of the pre-construction phase, including utility coordination, belongs to the Contractor. All Design-Build (D/B) Projects meet the determinations “in the best interest of the public” and “expedites the staging of the project”. As a Design Build Project, this project shall be automatic Public Interest Determinations and should include, in accordance with the executed Memorandum of Understanding (MOU), the utility relocation, removal, and adjustment work in the construction project as part of the Project scope.

General Utility Comments:

MOUs have been sent to the utilities. Have not received all MOUs back at this time.

Signing and Marking Plans:

- ⇒ 21 WB: Need to verify sight distance of the traffic signal in relation to the pedestrian bridge to see if advanced warning devices are needed. The Inspection Party discussed this and raising the bridge may be an option.
GDOT OID Response: Volume 2, Section 16.3.8 currently addresses this concern. The DB Team is required to ensure that the Proposed SR 21 Pedestrian Bridge “does not create any form of visual obstruction or interference with the traffic control devices at the Rice Mill Road intersection with SR 21.” Additionally, the GDOT design consultant has conducted a review to ensure that no visual obstruction will occur. A verification of this review has been requested.

- ⇒ SR 21 EB Need to determine if additional measures are needed at the traffic signal head in relation to lighting on the pedestrian bridge to reduce blending in.
GDOT OID Response: There are currently no requirements for lighting for this project. If lighting becomes required for the project, the language from this section can be clarified to include “lighting interference.”

BRIDGE INFORMATION

Existing Bridge ID No.: N/A

Proposed Bridges: 10' minimum clear width Pedestrian Bridge across all lanes of SR 21.

General Bridge Comments:

PM to meet with Port Wentworth City Officials to discuss lighting, bridge aesthetic, etc.

REQUEST FOR PROPOSALS

REQUEST FOR PROPOSALS / INSTRUCTIONS TO PROPOSERS

VOLUME 1

- ⇒ Recitals C. Verify the date. The Instructions to Proposers shows 4/22/16.
GDOT OID Response: The dates in this section will be verified and updated.
- ⇒ Recitals D. Verify the date. The Instructions to Proposers shows 6/17/16.
GDOT OID Response: The dates in this section will be verified and updated.
- ⇒ 2.2.1: Change NEPA to GEPA.
GDOT OID Response: Per guidance from an OES review of the RFP, references within the DB Contract to NEPA and/or GEPA have been covered by substituting the term “Environmental Documents.” This term has been defined in DBA Exhibit 1 of the DB Contract to mean, “all required documents and submittals pertaining to NEPA, GEPA, and/or state and federal environmental laws and permits which are necessary to complete the Project.”
- ⇒ 6.3.8.4(e): Remove the second “is” from the statement.
GDOT OID Response: The word “is” will be removed.
- ⇒ 13.1.1.4(b), 13.2.1.4(b), and 17.5.2.2: delete the “and” from one hundred and eighty.
GDOT OID Response: The word “and” will be removed from all instances of this term.

VOLUME 2

- ⇒ 6.1: Georgia Power Transmission advised they will not raise the transmission line in place over the pedestrian bridge. It will need to be relocated.
GDOT OID Response: This is accurate – at the time of distribution for this document, the utility company had informed GDOT that they preferred to raise the transmission line. They have since changed their approach and the RFP (and the concept drawings) have been modified to reflect this change.
- ⇒ 18.3.1.1.2 All Other Roads and Streets: the 2nd sentence begins with “side streets” but end with “cross streets”. Recommend replacing “cross streets” with “side streets” for continuity.
GDOT OID Response: The term “cross streets” will be revised to “side streets”.
- ⇒ 18.3.1.1.2 All Other Roads and Streets: Governmental Entity is referenced in the 2nd sentence. Recommend replacing Local Government Agency in the last sentence with Governmental Entity for continuity.
GDOT OID Response: The term will be modified as suggested.

VOLUME 3

- ⇒ Revise the numbering for Section 4, 7, 10, 12, 13, 16, 18, 19, 23.
GDOT OID Response: Numbering will be corrected.
- ⇒ Table 4-1 GDOT Led NEPA Approval: Revise all references to NEPA to show GEPA.
GDOT OID Response: Per guidance from an OES review of the RFP, references within the DB Contract to NEPA and/or GEPA have been covered by substituting the term “Environmental Documents.” This term has been defined in DBA Exhibit 1 of the DB Contract to mean, “all required documents and submittals pertaining to NEPA, GEPA, and/or state and federal environmental laws and permits which are necessary to complete the Project.”
- ⇒ 8.2.1.1. Pile Foundation: determine if referenced Special Provision should be 534.
GDOT OID Response: Special Provision 534 is the GDOT Special Provision for Pedestrian Bridges. Special Provision 520, as noted in the RFP, will be included as an attachment, as required.

- ⇒ 8.2.3 Pavement Design: Volume 2 Section 11.2.2 does not provide minimum pavement design as stated.
GDOT OID Response: Volume 3 of the RFP is intended to be Programmatic in nature. As a whole, this volume is intended to be consistent and unchanged for all DB Contracts. Volume 2 is intended to contain project specific scope elements. As the contract is written, since there are no requirements for pavement design in Volume 2, there is no requirement for the DB Team to incorporate associated items from Volume 3.
- ⇒ 10.1: Revise the 3rd paragraph to the following: There is no suitable place to bury existing bridge and/or construction debris within the project's limits. The DB Team shall provide an environmentally approved site to dispose of existing bridge and/or construction debris at no additional cost to the Department.
GDOT OID Response: This paragraph will be amended, as requested.
- ⇒ 18.3.1.1.2 Lane and Shoulder Closure During Design-Build Period: Verify that the restriction from Wednesday before Thanksgiving to after New Year's Eve is required for this project.
GDOT OID Response: As discussed in the RFP Review Meeting, this requirement has been used on other projects and is justified on this project due to the Project's proximity to the outlet shopping nearby.
- ⇒ 18.3.1.1.2 Holiday Restrictions: Thanksgiving Holiday and Christmas Holiday is already addressed in Lane and Shoulder Closure During Design-Build Period.
GDOT OID Response: The first instance of restriction, from the Wednesday before Thanksgiving Day to the first business day after New Year's Eve, defines restrictions for specific daily hours (5:00 am to 11:00 pm or 7:00 am to 11:00 pm respectively). The second instance of restrictions, for the Holidays of Thanksgiving and Christmas, defines complete restrictions for the entire length of the days, as defined. Since there is no inherent contradiction, no change to the RFP is required.
- ⇒ 19.2 Construction Maintenance Limits Plan: Revise the last paragraph to show Volume 2 Section 23.
GDOT OID Response: Volume 2 will be specified here.
- ⇒ Attachment 2-1: Attachment is labeled P.I. No. 0013367 SR 400 Widening Project. Please correct to show correct project.
GDOT OID Response: The text will be revised to reflect the name and P.I. of this project.

FIELD INSPECTION

- ⇒ The existing fence at the school will be cut for landing construction. Provide replacement fencing in kind including a gate if desired by the School Board.
GDOT OID Response: Fence replacement will be required within the scope of the RFP and will be shown on the concept layouts.
- ⇒ Add guardrail for the median if the center support is used.
GDOT OID Response: Volume 2, Section 11.1 of the RFP currently requires that, "Adequate Protection, in accordance with the DB Documents and its associated design manuals, shall be provided to the Traveling Public for all Pedestrian Bridge elements, Access Ramp elements, and stationary obstructions within the Project clear zone." This scope ensures that guardrail or some other form of protection is installed.
- ⇒ Ensure clearzone is maintained on the southbound side.
GDOT OID Response: Volume 2, Section 11 of the RFP currently requires, "The Pedestrian Bridge Access Ramps shall be placed outside the clear zone of the state route." Furthermore, Volume 2, Section 20 requires, "The DB Team shall design and construct the sidewalk in such a way as to maximize the offset of the sidewalk from the edge of travel lanes. The sidewalk shall be installed to the outside of the stormwater conveyance and to the inside of the Access Ramp." Clear Zone will be maintained.

PERSONNEL SUBMITTING E-MAIL COMMENTS

Allen Ferguson
Jeffrey Garnett

District 5 Utilities Office
Office of Environmental Services

c: Meg Pirkle
Hiral Patel
Darryl VanMeter
Andrew Hoenig
Cory Knox
Bryan Czech
Korey Murray
Joseph Capello
Troy Pittman
Cynthia Phillips
Dallory Rozier
Robert McCall
Michael Garner
Ross Etheridge
Charles Hasty
David Patterson
Dale Brantley
Lee Upkins
Andrew Heath
Christopher Raymond
Andy Casey
Anton Sova
Bill Duvall
Chris Rudd
Troy Byers
Cindy VanDyke
Amber Phillips
Gail D'Avino
Eric Duff
Meghan Hedeem
Glenn Williams
Derrick Cameron
Troy Patterson
Daryl Williams
Christy Lovett
John Kopotic

| | |
|--|---|
| Project Name PI No. 0013549 Chatham Co. Pedestrian Bridge Over SR 21 Near Rice Mill Road | Date of Meeting February 23, 2016 |
| Location GDOT, Savannah Area Engineer's Office 630 West Boundary Street Savannah, GA 31401 | Time 10:00 AM |
| Purpose of Meeting Utility Coordination Workshop | Participants (See attached) |

Meeting Minutes

A Design-Build Utility Coordination Workshop Meeting was held for the SR 21 Pedestrian Bridge Project at the GDOT Area Engineer's office in Savannah, Georgia at the time and date listed above. Discussion topics are summarized below with their corresponding agenda items.

1. Introductions

Allen Ferguson, the Senior Utility Coordinator representing Georgia DOT, began the meeting by distributing the Utility Workshop materials and by asking all the attendees to introduce themselves and state which organization they were representing.

A sign-in sheet was passed around to record the name and contact information of those in attendance. It has been attached to these meeting minutes.

2. Project Overview - Pedestrian bridge over SR 21 near Rice Mill Road

Jonathan Langley, the HNTB Project Manager representing GDOT, gave an overview of the project. He described the area surrounding the project site, pointing out Rice Creek School, the Publix Shopping Center, the Rice Creek Neighborhood, the site of the future pharmacy, and the wetland areas to the west of SR 21.

Jonathan Langley described the purpose of the project, stating that sidewalk and a pedestrian bridge would be constructed to provide connectivity from the neighborhoods to the west side of SR 21 to the Rice Creek School. He said that the current plan was that the school board would be providing right of way that currently belongs to the Rice Creek School and that they would be responsible for installing a sidewalk directly from the school to the bridge.

Jonathan Langley said that the bridge construction would include ADA compliant ramps on either side of the bridge and that the bridge may be either one- or two-spans. If it were to be a two-span bridge, a column would be required in road median. This column would require barrier protection.

Jonathan Langley stated that when the undeveloped property on the southeast corner of the SR21 and Market Blvd. is build, the developers would be responsible for sidewalk installation / connectivity in this area.

Jonathan Langley also discussed the Design-Build process and the Design-Build contract. He said that this project would use the One Phase Low Bid selection method. This is a streamlined version of Design-Build procurement that does not use a separate Request for Qualifications step nor does it request / evaluate Alternative Technical Concepts.

Jonathan Langley opened the floor for discussion relating to preferred utility relocation approaches, based on the project description and layout provided.

Rick Long, with Georgia Power, indicated that, based on the information provided, the preferred method of relocation for the Georgia Power Transmission lines would be to raise the lines above the bridge. This would likely require the relocation for the other utilities currently sharing the poles to relocate underground, including Georgia Power Distribution. Rick Long indicated that additional information, such as project constructability techniques, would need to be provided / evaluated in order to fully confirm this method of relocation. He also stated that as many as two additional poles, in advance and beyond the project site, may need to be adjusted to raise the line.

Rick Long said that other alternatives included boxing out the lines and even rerouting the power. In order to box out the lines to go around the outside of the construction, acquisition of easements, acquisition of permits, wetland impacts and other factors would need to be considered.

Jonathan Langley inquired how much clearance would be required between the bottom of the transmission lines and the top of the bridge. Jonathan Langley stated that the overall bridge height would be approximately thirty (30) feet tall. Rick Long said that, at a minimum, a fifteen (15) foot clearance would be needed. He also stated that approximately five (5) feet of clearance would be required if the line were to be buried.

Jonathan Langley asked if any other offset requirements were available. It was stated that four (4) feet of offset was traditionally required for gas lines. It was also stated that offsets may be required between each utility.

Jonathan Langley inquired as to whether any of the utility companies in the meeting were aware of what the unknown utility in the median was. No one knew.

Trent Long and Brian Harvey, with the City of Port Wentworth, inquired wither any architectural or aesthetic elements were going to be included within the project scope. Andrew Hoenig, the GDOT Project Manager, said that at this time that was not part of the planned project. He said that if it was desired, the City could make a formal request. The City indicated that it would like to do so and would provide the request.

The issue of seasonal times for relocations was briefly discussed. It was stated that these seasonal requirement would be provided within the forms that each utility would be submitting back to GDOT.

Johnathan Langley advised the bridge will have lighting.

3. Project Schedule

Jonathan Langley outlined the key project schedule milestone dates, provided in the agenda below. He described the importance of promptly receiving the Utility Owner forms (MOUs and UAMs) prior to the release of the RFP so that all the proposers would have the appropriate information for bidding the inclusion of any utility relocations within their proposals.

- **Memorandum of Understanding and Utility Analysis due back 03/24 /2016**
- **GDOT issues Request for Proposal (RFP) 04/15/2016**
- **GDOT letting (receipt of bid/technical proposal from Finalists) 06/17/2016**

4. Memorandum of Understanding (MOU)

John Tuttle, representing GDOT, walked through the MOU in depth by describing each section. He outlined how the MOUs fit into the Design-Build process and how the utility companies would fill out the form, depending on which approach they preferred for their relocations.

5. Utility Analysis

John Tuttle also gave a detailed description of the UAM and explained the importance of the information for the proposers to ensure that all teams were providing accurate cost estimates for the Design-Build utility relocations to be performed.

6. What to Expect with Design-Build

John Tuttle gave an overview of the Design-Build process and briefly described each of the components of the process in the agenda below.

- **RFP**
- **Design-Build Contract**
- **Contract Award and Utility Kickoff Meeting**
- **Design Utility Relocations (for those in contract)**
- **GUPS Responsibility**
- **Monthly Coordination Meetings**

7. Open Discussion

Hargray and Georgia Power both indicated that they believed that they had facilities within the project limits that were not included in the SUE layout provided within the meeting. Allen Ferguson indicated that they could provide markups of the layout to GDOT in order to indicate where those facilities were.

Allen Ferguson asked the City of Port Wentworth if any waterlines were present in the area of the pedestrian bridge. The City indicated that there were not any lines present there.

The City of Port Wentworth indicated that they would not likely be able to get their city council to approve the relocations until the March 24th meeting. John Tuttle indicated that they could provide GDOT with a draft proposal of what they would be presenting to the board and this could be provided in the RFP until an approved version was acquired. He indicated that this has been done on other projects.

8. Closing Remarks

- GDOT appreciated the turnout and input from utility owners and looks forward to successful utility coordination and teamwork to get this project completed on schedule.

9. Action Items

- Each Utility Company is responsible for completing the Memorandum of Understanding and the Utility Analysis and for returning it to GDOT. GDOT indicated that they would prefer the return of the forms by March 15th, however they must be returned by March 24th, as indicated above. Three signed original copies of the MOU and one copy of the **Utility Analysis shall be mailed to Allen Ferguson at:**

**Georgia Department of Transportation
District Utilities Office
Attn: Allen Ferguson
P.O. Box 610
Jesup, GA 31598**

Or Physical address:

**204 North Highway 301
Jesup, Georgia 31546**

- GDOT said that they would task their designers with investigating concept solutions that would not require large equipment to be used beneath the active transmission lines in order to place / construct the bridge spans.
- The City of Port Wentworth expressed interest in requesting that aesthetic / architectural elements be included in the scope of the bridge design. The City said that they would generate and transmit official requests for those elements to GDOT.
- GDOT will attempt to obtain development plans and schedule for the possible development of the property in the southeast corner of SR 21 and Market Blvd.
- MOU/UA package will be sent to Georgia Power Transmission and Atlanta Gas light as they were not able to attend the meeting.

The notes contained herein are our understanding of the topics discussed and the decisions reached. Please contact Allen Ferguson at (404)-877-8939 or aferguson@dot.ga.gov or Jonathan Langley at (404) 623-0964 or jlangley@hntb.com with any questions, comments, or concerns.

